

THE **S** **RESOURCE** **SCALE**

NEWS, REVIEWS, INFORMATION TO USE

April/May 2017
Volume 3 No. 4



***Lehigh Valley Models Sandhouse
Make A Low-relief Factory Wall
Finishing the Don Winter's Railgon Kit
Bill Winans' St. Louis Eastern and Pacific
& Coal Creek Central
Shows, Meets and So Much More...***



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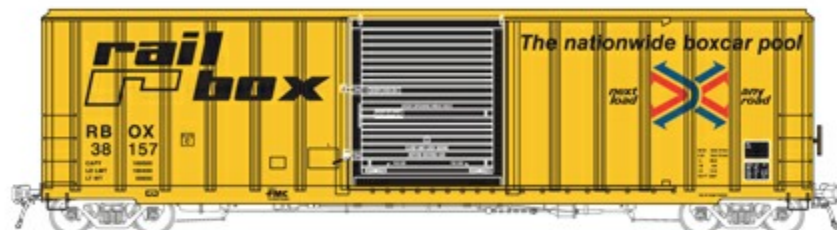
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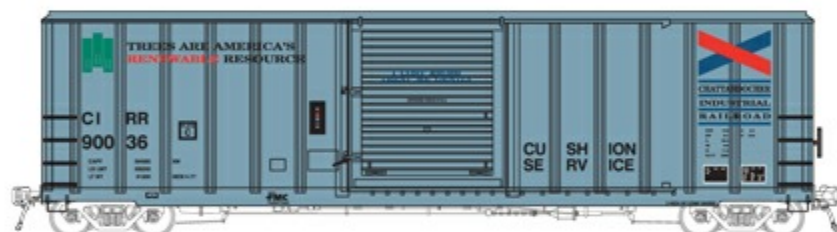
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April/May 2017

Volume 3 No. 4

Welcome to the online *S Scale Resource* magazine. The magazine is presented in an easy to use format. The blue bar above the magazine has commands for previewing all the pages, advancing the pages forward or back, searching to go to a specific page, enlarging pages, printing pages, enlarging the view to full screen, and downloading a copy to your computer.

Front Cover Photo

Loco servicing facilities in Tompkins. A scene of Bill Winans' St. Louis Eastern and Pacific & Coal Creek Central layout that continues to be worked on.

Inside Rear & Rear Cover Photo

When we travel we are always taking pictures for possible modeling. While in Arizona, we spent time in Jerome. Pictured is the Audrey Headframe.

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The Model Railroad Resource LLC publishes ***THE O SCALE RESOURCE*** and ***THE S SCALE RESOURCE***. Be sure to look at both of our magazines. There are many articles in our magazines that are not scale specific and will be of interest to you. Click the magazine title in this announcement to see the magazine.

From the Publisher's Desk



Last issue, I asked our readers to submit photos, comments or articles to us so that we could share them with everyone in the hobby. Thanks to those readers that heard my plea and did just that! Be sure to check out the *On the Workbench* by Karl Johnson and the *Workbench Extra* by Gaylord Gill. These are the types of submissions that we get excited about and enjoy featuring. Keep in mind that even partial projects are better than none at all. In this issue, Jim Kindraka shows us how he built loads for the Don Winter's gons that were featured in the [June/July 2016 issue of *The S Scale Resource*](#).

With respect to layouts, they aren't built overnight. We want to shoot and feature layouts – started, finished and everything in between. As you will see, Bill Winans' is a work in progress like many of our reader's layouts. We at the Model Railroad Resource like to show these stages in the life of a layout. While beautifully finished full basement/room layouts are nice to look at, they had to get that way. Those are the things we also like to see and visit. It is great to see what others are doing, share tips and techniques, and explain the process at a current stage or along the way. Please keep your photos, comments and articles coming. And, if you have a layout you'd think other modelers would enjoy learning about, let us know. If we're in your area, we'd love to come visit so we can write an article. If we can't get together, we will work with you to put your pictures and story into an article. Contact us at:

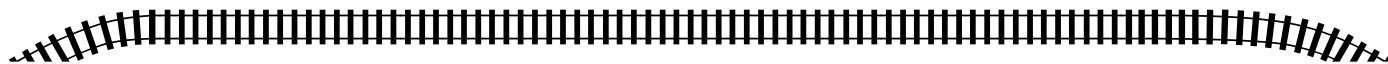
amy@modelrailroadresource.com or daniel@modelrailroadresource.com

This issue also features Mark Charles who talks about building fronts for shelf layouts. He mentions using local resources like community colleges or Makerspaces (see article for explanation) for certain aspects of modeling such as laser cutting. Students are always needing to complete assignments, and many times are eager to work on something different. Trade schools can also be a great resource, especially for young people that may want to expand their talents beyond what they're learning in school by applying it to the real world. Plus, by showing young people what can be done with their talents, you may be inspiring a future model railroader or manufacturer.

Happy Reading & Happy Modeling,

Amy Dawdy

NEWS YOU CAN USE



Bill Mosteller from greatdecals.com says, Decals for Norfolk and Western class H-9 and H L 2-bay hopper cars (set # 138), in white, are available in S-scale from Great Decals, 3306 Parkside Terrace, Fairfax, VA for \$7.75 each, postpaid. Each set provides 12 "canned" road numbers, and the modeler can easily make any road number valid for the cars. Each set does one car. The decals allow modelers to produce many uniquely numbered, scale N&W 2-bay hopper cars.

These decals include the 24" N&W herald, road name, road numbers for both sides and ends, N&W car classes, repack stencils, dimensional and end data specific to these cars. Alternate LT WT lines are also provided. Virginia residents please include sales tax. See our web site, www.greatdecals.com

[Concept Models](#) announces the release of their entry into the S Scale market with the Combustion Engineering CEBX 101 schnabel car. The car alone is priced at \$99.99. The car is easily assembled with ACC cement (Krazy Glue). The unit is sold less trucks and couplers. The 6-truck design should function well on standard S Scale layouts with standard curves.



In addition to the car kit, two optional loads are available; a boiler load at \$49.99 and a pressure vessel load priced at \$69.99.

The company plans to release their entire line of depressed flat and schnabel cars in the future. The model consists of resin castings and assembly hardware. Hand grabs, ladders, and related metal detailing are not provided. Focus is on providing the basis for the modeler to exercise his skill in taking the model to the detail level desired. Instructions are all photo illustrated by an experienced technical writer. The company currently markets a variety of models available at web address www.con-sys.com



Dave from [LBR Enterprises, LLC](#) says, One of our newest "S" items is a cast white metal raised water hatch. Size is .867" L x .371" W x .385" H



\$8.25 ea. with FREE SHIPPING via USPS in CONUS. [See their Website](#) for more details.

For those who are interested in the Maine Central Railroad and obtaining decals for that road, Bill Morris from the Bristol S-Gaugers in Massachusetts has developed artwork along with logos from himself for the “1950s-mid 1960s” green and gold and maroon and gold striped schemes. He originally developed the striping for the Boston and Maine. Since the B&M and the MEC shared the same management structure into the early 50s their road schema matched accordingly but with different logos.

The decals will match road engines: F3s, E7s, SW7-9s, GP7s, RS3s. For information on set pricing, availability and when to order (they are weeks away) contact [Bill Morris](#).



These Locos will include a Loksound Select decoder with a 21MTC interface capable of up to 8

light functions. While they will be set from the factory, they will have adjustable Chuff Rate using simple to control CVs. Out of the box, there will be very little to do but set the loco number. These will include all brand new sounds as mentioned above and will be set up to have "all the bells and whistles" of the prototype!

To see more about this exciting “Full Throttle” Loksound decoder [look here](#).

To order your reserved model with factory installed Full Throttle DCC with sound, contact [River Raisin Models](#) now.

Al Castellani from East West Rail Service says: East West Rail Service will be adding blinds for American Models Vista Dome car to our product line. We already offer blinds for the Budd RPO, Coach, Diner and Observation car. The Vista blinds will be available in the next few months and will sell for \$6.00 a set. I have included an image of the Vista Dome car with the blinds installed.



See all their products at eastwestrailservice.com

Project Update: Boo Rim Precision has given us a preliminary schedule that shows the sample models will be sent to RRM by the end of April 2017, with planned delivery of the production models by the end of July 2017.

Additional Loksound Information:

ESU LLC is pleased to announce many brand new recorded Full Throttle Steam Files starting to be released in February, 2017! After recording nearly 15 steam locomotives in 2016 alone, those efforts are finally making it to the mainstream of DCC and Sound!

These new sound files will include many new Chuff sets, new Whistles, Bells, Rod Clank and other Drifting sounds, Air Pumps, open cylinder cocks, and many other appliances. To pull them together for true realism like nothing else available on the market, we added our new “Full Throttle Steam” features. You can now be in control of whether your loco is pulling hard in intense chuff, or drifting with the rods clanking away at any speed! No set up or difficult calibration is needed, just simply press the new “heavy load” function button and let the fun begin!



[River Raisin Models](#) offers Factory Installed DCC with Sound. River Raisin Models is proud to be working with ESU to create a special Full Throttle file for the Upcoming

Berkshire locomotives! We will now be offering the production models with sound and DCC already equipped.

[BlueRail Trains](#) has just added Consisting and Speed Matching to our iOS app. This speed matching utility gives you a nice interface that's intuitive and almost fun to use, so anyone can accurately speed-match their locos in minutes as you run them on your layout. BlueRail boards have been very popular in S and O locos. BlueRail has also recently lowered their prices and offer volume discounts on their boards.

Here is a [video demonstrating speed matching](#).

Here is an [article about discount pricing](#) on BlueRail boards.



[Precision Vintage Classics](#) announces three new kits.



First we have released our Bryan Ellerby design ore car in On30 and Sn3 with trucks & couplers, no decals. Kit numbers OK23 and SK23 priced at \$38.00 plus shipping.

We also are releasing an A Frame Kit for a Bachmann part #26531 On30, 18' 3 board gondola.



This will convert a regular gondola to an A-Frame gondola. This is kit OK13 at \$5.00 + shipping. The kit consists of a resin A-frame casting, hinges and hinge rod. The kit does not come painted as the color is left up to the individual modeler's taste.

In other news we are going to Flat Rate Shipping. \$15.00 for Domestic and \$25.00 for International. [Check their refreshed website here.](#)

The S Scale Resource April/May 2017

[Tichy Train Group](#) has some new S parts. Right angle vent pipe # 3554: This is a right angle vent for coming out of the side of a structure. It includes the vent pipe, flashing & tapered stack cap. Pipe is 12" diameter by 2' x 4'. The end of the pipe is hollow.



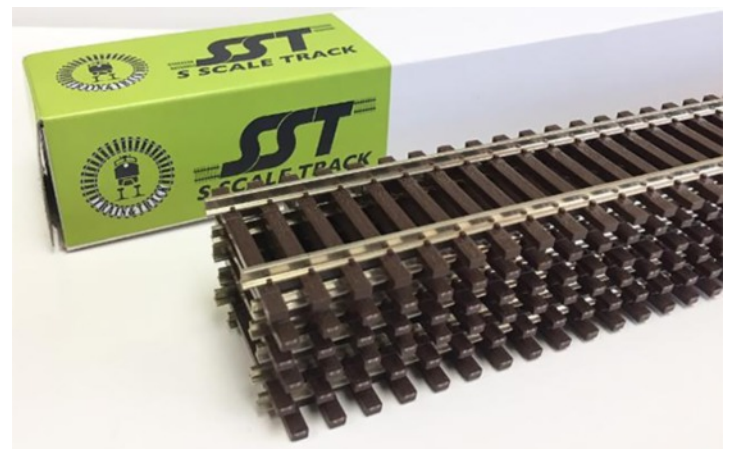
#3554

#3555

Pipe can be extended with kit 3555 which is a straight vent for coming out of the roof of a structure. It includes the vent pipe, flashing & tapered stack cap. Dimensions are 12" diameter by 8' long. the end of the pipe is hollow. Pipe can be used with kit 3554.



A new S Scale Track system is being imported by [Fox Valley Models](#). It features Code 138 Nickel Silver rail (which will match up with a few others out there) with plastic ties injection molded in brown. They will be starting with Flex Track, #5 Turnouts in either Scale or Hi-Rail versions, along with standard metal rail joiners, insulated joiners and terminal joiners. Other pieces are planned for the future. If there is something you would like to see, please let them know.



[Ron Sebastian from Des Plains Hobbies](#) says they have the new Fox Valley Models code 138 flextrack, and rail and terminal joiners in stock.

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Finishing the Don Winter's Railgon Kit or Loads For Your Gondolas!

By Jim Kindraka

In the [June/ July 2016 S Scale Resource](#), I wrote about building the Don Winter's "Railgon" kit. That article stopped at the point of painting the car. After boxing everything up to move to a new home, I finally dug the unpainted car back out and began work to finish it. This is a short article on how these projects can get away from you and take on a life of their own. I suppose the fact that I paid very little for the original kit spurred me to keep adding on.



This is the completed Winter Railgon kit after painting and lettering. The decals were made many years ago by G&W Models, but went on without issue. The website rrpicturearchives.net has a multitude of photos that can help with decal placement. This model was photographed on the late Jack Sudimak's layout in Medina, OH; courtesy of the Northeast Ohio S Scale group who faithfully maintain Jack's large S Scale layout.

In the June article, I talked about doing some research and adding underbody and brake rigging to the car. I was able to obtain some decals to finish the car in the GONX “Railgon” lettering scheme, so I masked and painted the model in that scheme. Despite being 20+ years old, the decals went on easily using Micro Set and Micro Sol decal setting solutions. The decals fit the gondolas sides almost perfectly. The original decals were done for the Winter’s kit by G&W Models; a small S Scale business in Syracuse, New York run by the late Walter “Walt” Danylak. In the 1970’s through 2005, G&W imported a single brass model and made several other limited run products, as well as detail parts and decal sets. All the decal sets I have seen or used have been of very high quality even though they are now quite old. When Walt passed away in September, 2005, the business was not continued; an unfortunate loss for the S Scale community.

Following decal application, some touch up painting and addition of a few final brake details, the model was finished with Kadee 802 couplers and a pair of American Models 70-ton RB trucks. The trucks were modified to come as close as is possible to 100-ton trucks in S Scale receiving 36” Code 110 steel wheels.

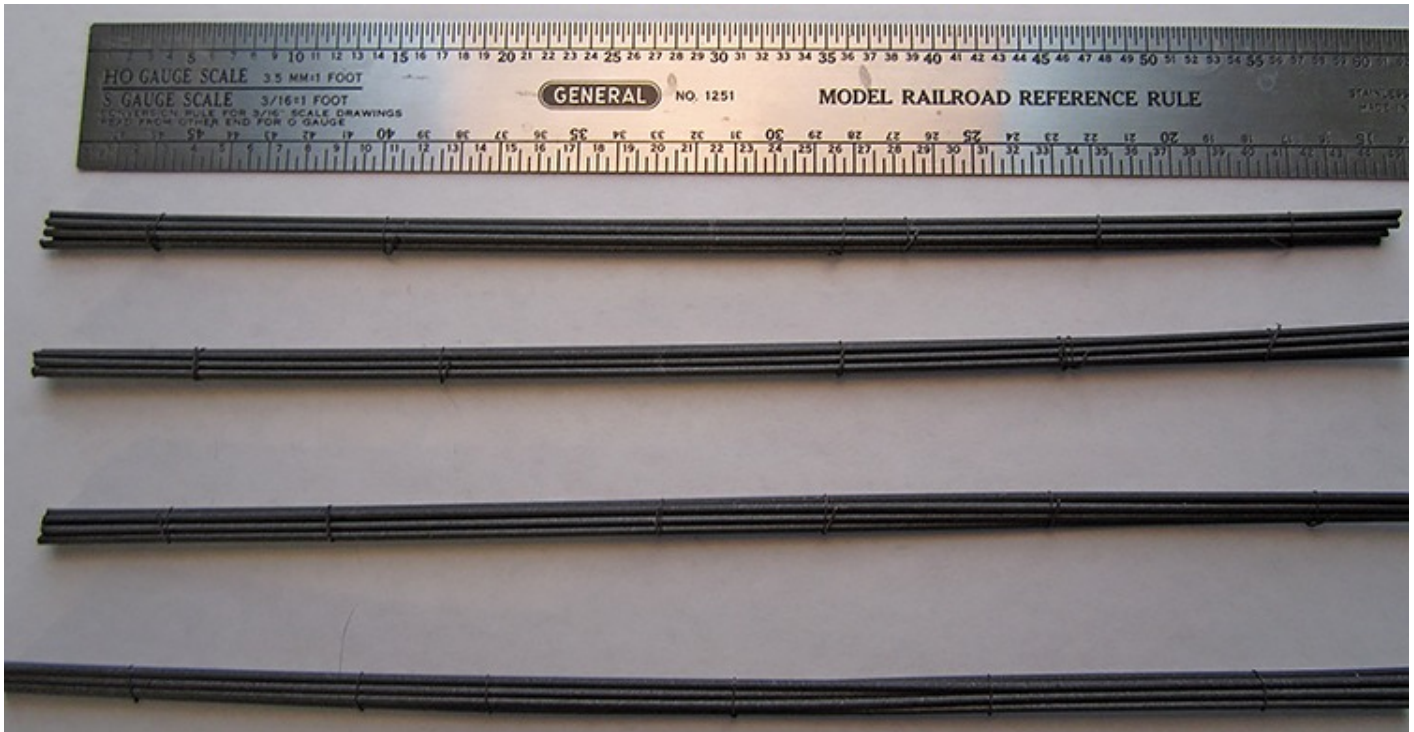


These are photos of a couple flat car loads purchased by a friend at Train Fest in Milwaukee. They are made by a Czechoslovakian company and are available in the US through a distributor in Maine. These loads led me to do some on-line searching of the distributor for likely gondola loads.

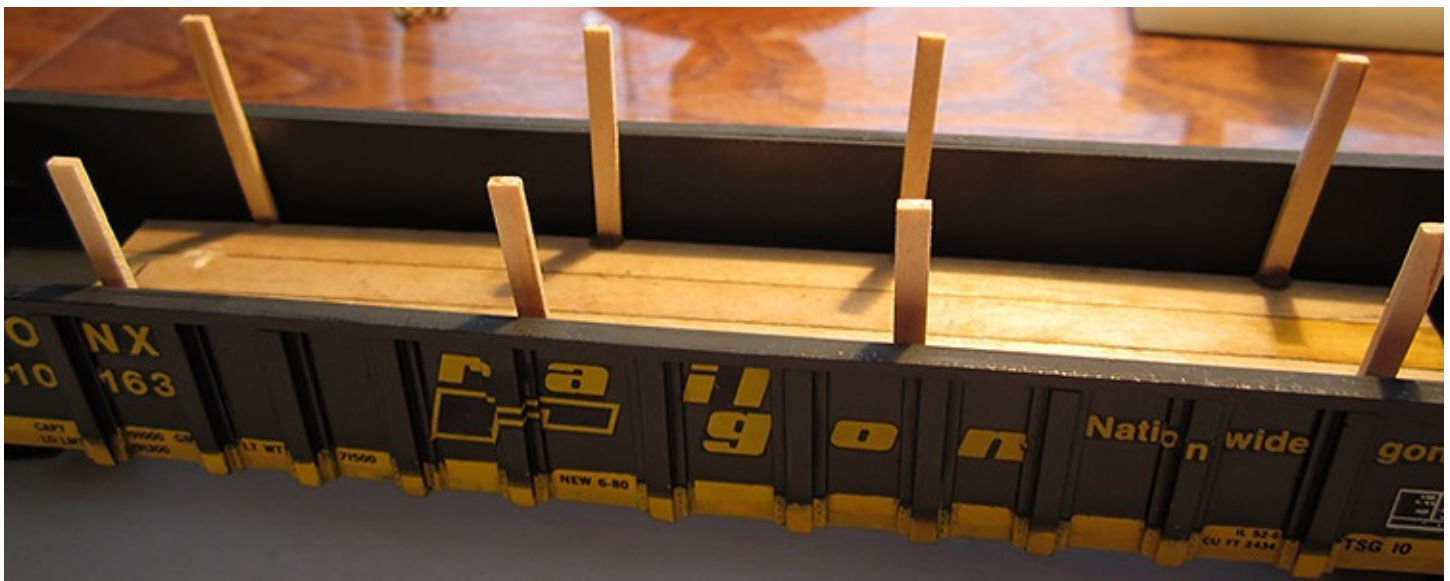


Then things took a bit of a sideways turn... At this point, other than time, I had very little invested in the model so I began to look for a suitable load. I wanted to do something different, not a simple load of scrap or gravel, but something a bit more unique.

What the heck, I hadn't spent much to date so why not do some looking and spend a few bucks! A friend showed me some HO and S Scale flat car loads he purchased at Train Fest in Milwaukee. They really looked interesting so I began looking into where they came from. The items were made by a Czechoslovakian company named Duda and sold in the U.S. through JWD Premium Products in Liberty, Maine.



The photo above shows the item I purchased, Steel Rod Bundles for a 68' HO car. The package contains 10 bundles and they work out to 45' in S Scale. The rods are black styrene and come out of the package bundled exactly as you see in the picture.



To hold things together, I built a simple cradle from ties and leftover pieces of laser cut wood. I elevated it off the gondola's floor about 3/16ths of an inch to reduce the number of bundles needed, but still giving the sense of a fully loaded car.

The JWD Premium Products web site listed various loads in HO, S and O scale. The majority were obviously in HO, but there were some nice S scale offerings. However, I became interested in an HO product that consisted of bundled steel rods. I've seen these before while rail fanning especially in the upper Midwest

steel producing areas. Bundled rods were available for a 68' car in HO. The actual rod bundle works out to just over 45' long in S scale which sounded promising for the 52' Railgon so off went an order.

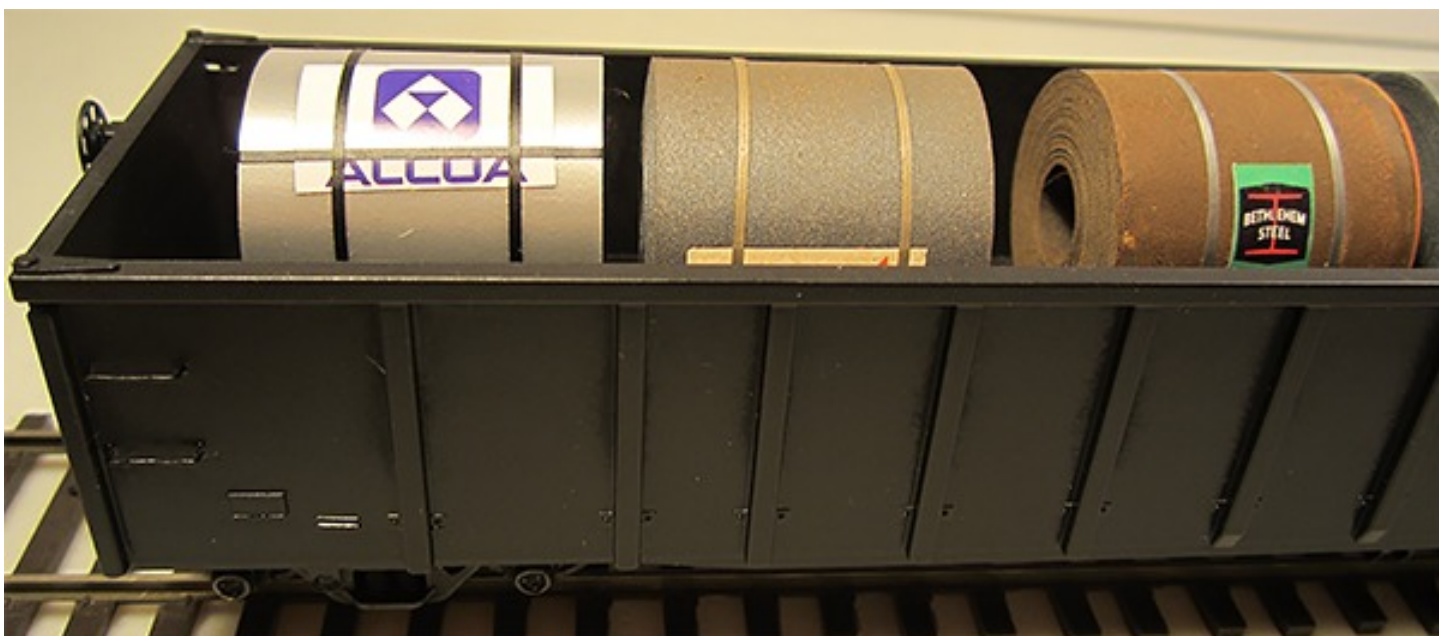
The bundles turned out to be just the effect I was looking for. I built a cradle out of scrap scale lumber and went to work. Of course, one package wasn't enough which led to two, and three, and ... Before I was done, I had more invested in the load than the car! Like I said, sometimes things take on a life of their own. I've tried to provide some photos so you can see the effect. S Scale has several flat cars, and a recently introduced Thrall 2743 gondola kit from S Scale America made finding these loads timely. They provide the modeler with many ways to get creative and add flavor to trains and interest to model railroad operations.



Here is the finished car with several packages of steel rod bundles for a load. It is definitely a case where I ended up with more invested in the load than the gondola model! It does provide a little different, though quite credible load, for more modern freight trains. I may yet finish it by drilling a small hole in the top of the upright wood and using some black thread to simulate wire banding. Another interesting option would be to slant the wood uprights slightly, along with the rod pile, to model a load that has shifted somewhat in transit.



Another view of the finished car with several packages of steel rod bundles for a load.



I recently completed a couple of the new SSA Thrall 2743 gondola kits. The cars are quite simple to construct, and I am once again searching for unique loads. This is a photo of one of the gondolas with three different S Scale steel coil loads made by the same Czechoslovakian company that makes the steel rod bundles. I'm still deciding whether the Railgon will be accompanied in trains by a few steel coil gondola loads.



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WILLIAM “BILL” WINANS’ ST. LOUIS EASTERN AND PACIFIC & COAL CREEK CENTRAL

By Amy Dawdy

Photos Daniel Dawdy (unless noted)

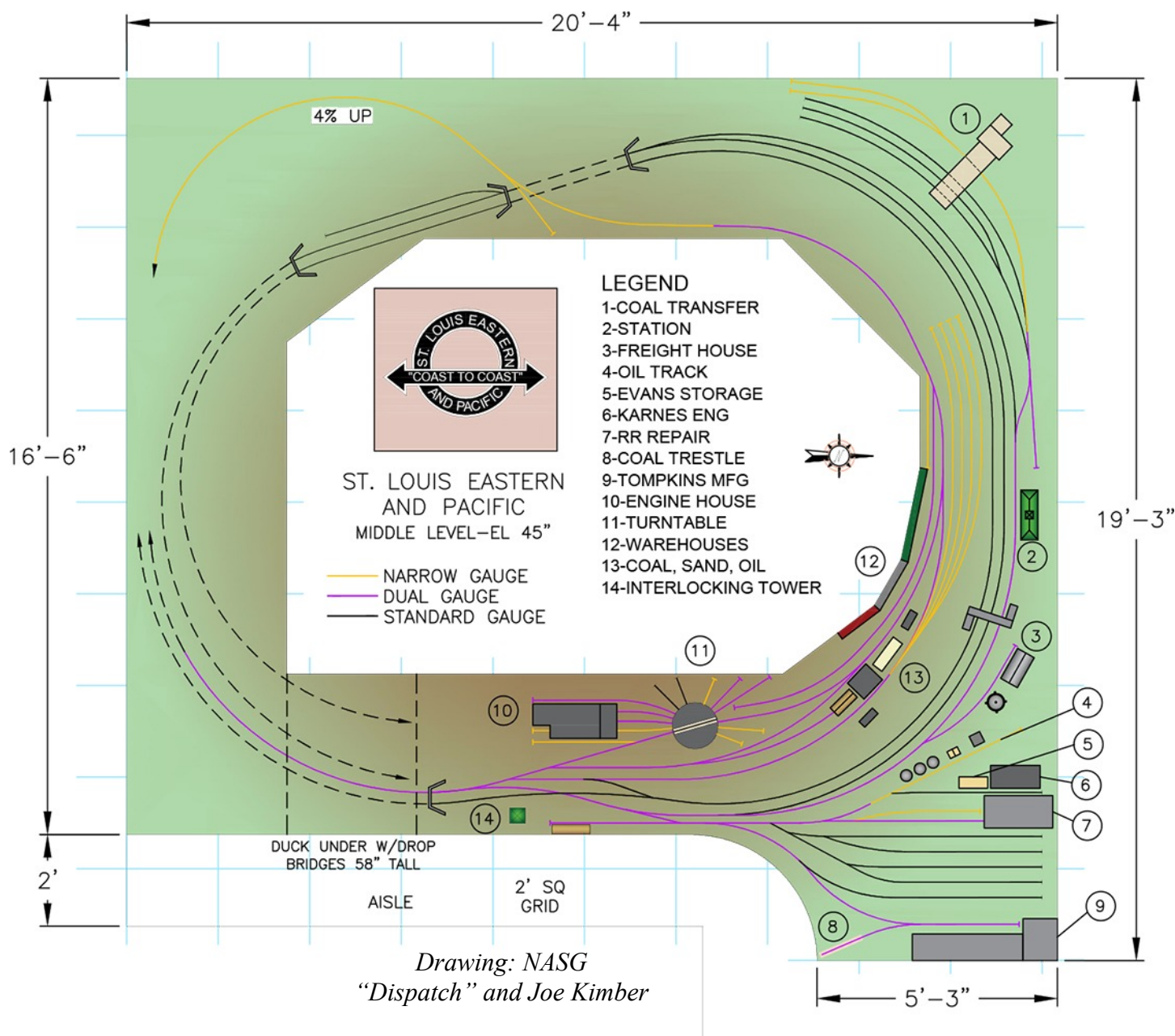
While on a recent trip to Arizona, we had the pleasure of meeting Bill Winans and his wife, Sandi. They were gracious enough to not only invite us into their home to see the St. Louis Eastern and Pacific (SLE&P standard gauge) and Coal Creek Central (narrow gauge), but they also took us to visit Steve Hatch. We will feature Steve’s layout in an upcoming issue.

Before we get into the specifics of the railroad, let me give you some background on Bill. When asked how he got started in model railroading, Bill replied “I was given trains before I could walk and they just got more and more sophisticated as I got older.”. He went on to tell us that his dad initially got him started in model railroading, and although not a model railroader himself, he did provide them to Bill until the age of 12. Bill said after that he was on his own. Bill was inspired by the model railroads in the model magazines, and said specifically the very small, HO Crooked Creek RR in the July 1959 issue of *Railroad Model Craftsman* has been the inspirational source for all his layouts since that date. He also told that the West Bay Model Railroad Club of Menlo Park, CA provided him with a good feel for what could be done.



The cute little #18. A P-B-L SP 4-6-0 (Sn3) with the tender from P-B-L DRGW C-18 #315. One of Bill’s favorite tender changes.

Bill always thought S scale American Flyer was more realistic than his Lionel 3 rail. When he saw what Henry Sprague of the Menlo Park club did in S, that became his goal, though it took many more years for that to happen. He got started in S when Bill Peter of P-B-L imported his first Sn3 DRGW K-37 2-8-2 in 1979-80. Bill had become a bit frustrated with the performance of his high end brass HOn3 locos, and when he saw the Sn3 loco, he was immediately hooked, trading away his HOn3 for Sn3 as like locos became available. As happens a lot in life, family obligations come first, and he lost his train room to a new baby son. He did, however, take the loss of the train room and turn it into an opportunity and made the transition from HO to scale S even though he had no place to build a layout. Eventually, when a grown daughter left home, he got a new train room and built his first scale S railroad.



Track plan for the St. Louis Eastern and Pacific portion of the Layout

Bill does not model a specific prototype with respect to trains or location, but said he does follow real railroad logistics. The narrow gauge Coal Creek Central is a feeder line to the standard gauge SLE&P. There are only two significant locations on the layout: the town of Tompkins where the two gauges meet and interchange, and Coal Creek at the end of the narrow gauge. Coal and oil from Coal Creek make up the primary traffic with merchandise and mining supplies back hauled to Coal Creek. The majority of the standard gauge is pass through traffic, which Bill said “is a reflection of the PRR/EBT”. Bill stated that from previous experience he found that modeling the real thing is virtually impossible, so rather than doing the impossible, he models something logical and at least semi realistic. That being said, his railroad is located east of the Mississippi River in late summer 1956. Not much scenery has been completed yet, but he will use a variation of hardshell. Bill said that the large curves are his favorite thing on the layout, along with flowing track work that “shows off his models in a realistic setting of very nearly prototypical curves.” See Bill’s picture below.

As mentioned previously, Bill has used the Crooked Creek RR design concept for a long time. It is essentially a loop with a branch line. Even though it has gotten bigger and more complex over time, it is still the



same concept. Once he had the concept, he developed the track design. For Bill, this starts with scenes he would like to have on the railroad, and laying them out on paper. All model railroaders know that with larger scales, room size dictates what scenes will fit, and which ones make the most sense. Since Bill’s layouts have all been based on the same concept, each one has been built upon and become an outgrowth of the last. The only the real difference for Bill is the space available and how it impacts the possibilities.

Bill said that much of the previous layout (buildings especially) gets incorporated into the new layout. In the case of his current layout, the whole old table section of the narrow gauge town

of Coal Creek was integrated into the new design. The greatest thing about his current layout is that it is in three times the space of the last layout, allowing Bill to add storage for ten whole trains on the lower level as opposed to two on the old railroad. It also allowed him to use much larger curves and shallower grades while increasing the space between levels.

Another thing Bill likes to do is modify his buildings and trains to suit his vision of what they should be. Very seldom do things go on the layout without some change from their original design. The most obvious changes are the re-tendered locos which he swaps or changes out to create a “family look” to his roster.

Below are before and after pix of Bill’s 4-8-2. With respect to this engine, Bill said “My understanding is that there were only FIVE of these green boilered versions made, so I would guess that some purists might be rolling over in their easy chair. My 2-10-2 got similar treatment.” Three pictures below by Bill Winans.



The new station below is a major redesign of the Oxford station from HRM Laser. You can see the original in the April NASG “Dispatch” or on the [HRM website](#). Bill stated that even D. Scott Peterson of HRM said he barely recognized the core kit in the station.



Station as photographed by Daniel Dawdy in December, 2016



Bill's updated picture of the station. He has been busy.

The actual construction of the layout was based upon Bill's need to climb on the structure; therefore, 2 x 4 lumber was used for all of the framing. Otherwise it is a common open grid and table top design. The whole track plan was driven by the fact that he wanted large radius curves.

When asked if the layout is DC, DCC or battery power, Bill replied that “while the layout's wiring is set up for DCC, I have only been running straight DC. It's more forgiving of momentary shorts caused by rolling stock or something else. Once everything else is done and I know all potential shorts are eliminated, I will probably convert to DCC. Battery power is an option for some of the switching locos.” It sounds like Bill uses what works for him based on what stage of layout building he's in.



The 204 and 215 are Omnicon MoPac 2-8-0s. Bill swapped out the original tenders with small USRA versions from OMI. The paint and cab interiors were done by Bill Shiverdecker, now deceased. The original tender went behind Bill's OMI 0-8-0 (photo, also painted by Bill) which gave up its tender to one of the 2-8-0s.

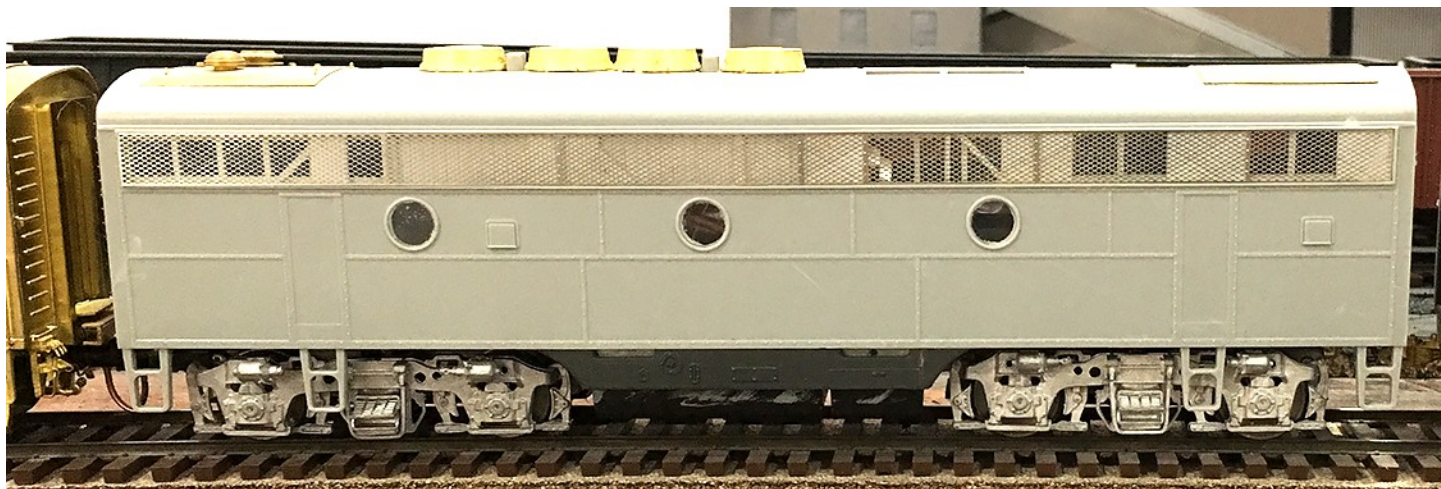
Above photos by Bill Winans



Bill Winans Photo

The F units above and on the next page are a hodgepodge of parts and pieces. The shells came from Wabash Valley (improved Enhorning shells), the chassis are cut down American Models FP-7 as are the blind ends and fuel tank skirts of the units, the motors are "Holland" coreless, the sideframes are copy cast Sunset Models (with permission from Mort Mann), the grills and roof panels came from Bill's OMI brass F-3A/B, the fans are from B.T.S., and all the details behind the grills are Evergreen styrene. Someday, Bill will add grab irons, door handles, lift rings and "glass" (that is already cut), but he needs to paint them first. He is still hashing out the paint design, so the final finishing of these is still a way off (after already putting in at least 25 years hauling freight!). Bill says they run much better than any of his brass diesels, and he relies on them to get

the brass F-3s off the mark. If you look at the individual units, they are all different F7A, F7B (late with 48" fan and Farr grill), F3B. The details behind the grills are specific to each unit. It took Bill a long time to find out all that info! Take a look at Wabash Valley on the NASG website if you want to see how these units started.



Bill Winans Photo



Bill Winans Photo

We always like to ask the modeler what their favorite part of the layout is. Bill said “It would be easier to list the things I don’t enjoy, but I think running the trains is the payoff. It helps a lot that the scale S trains are easy to see and appreciate their detail and running qualities. I like to “railfan” the layout – watching the trains go by is more fun than actually controlling them or working some operation. I can watch the same train go by over and over again. It’s like watching an instant replay of a particularly good move on the football field.”

Let's enjoy Bill's layout in pictures. The number on the image represents the approximate area of the layout based on the drawings earlier in this article.

Thanks again, Bill, for showing us your layout and chauffeuring us around Arizona!



Coal Transfer/Prep Plant. Narrow gauge coal in, standard gauge coal out.





1

Last shot of the coal transfer.



3



3

Coal Creek Central Freight Station built by Gerry Evans. Bill will do some new signage for it eventually. (formerly a fruit packing shed). The second building is Coal Creek Storage, also built by Gerry.



Above: Diesel fuel tank.

Below: The industrial area in Tompkins. On the far left is Evans storage, and Fraley's Car Rebuilding with the track inside. At the very far right, lower corner, is the local coal dealer's trestle.





7

Above: Loco servicing facilities in Tompkins with the Flatulene Oil Co and other buildings of Tompkins in the background. The 2-8-0 is an Omnicon MP 2-8-0 with a USRA tender.

Below: Overview of the Tompkins industrial area.



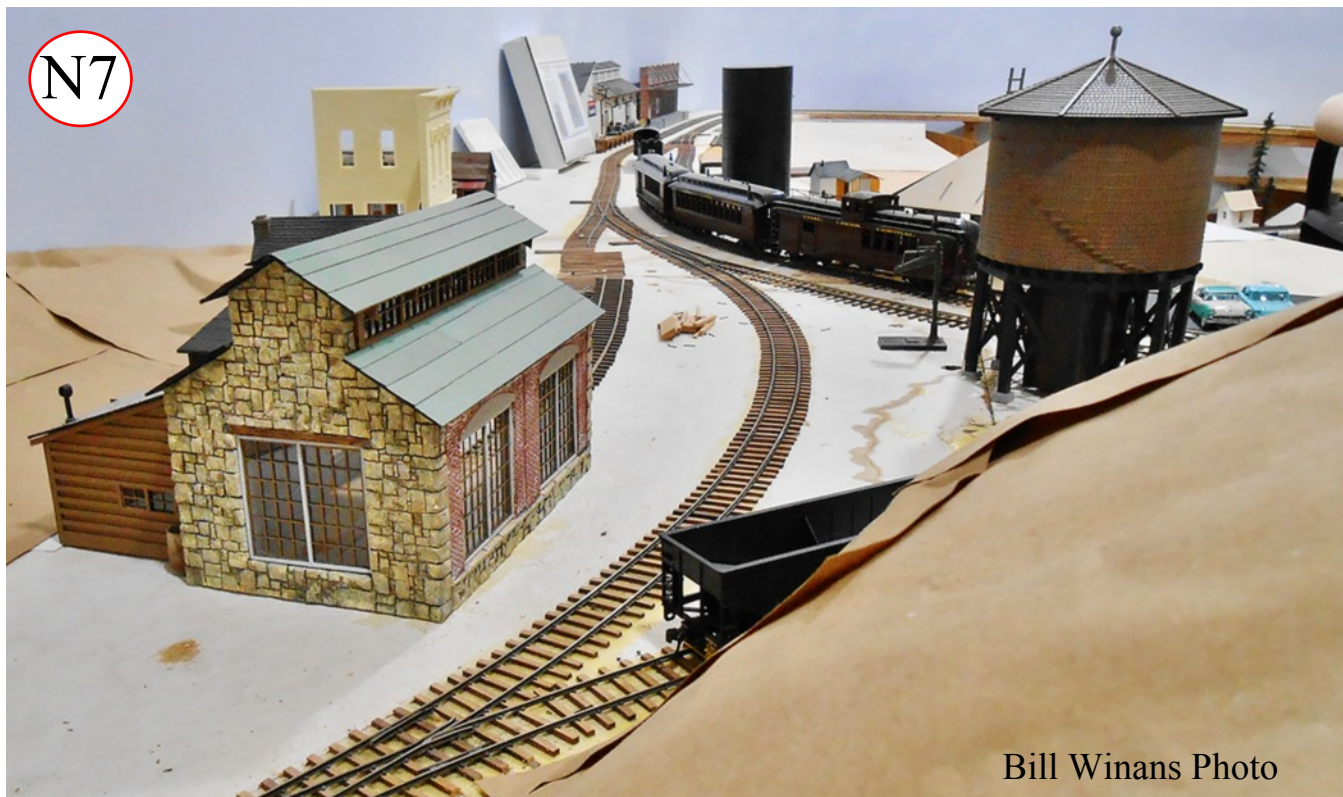
9



Above: The turntable at Tompkins. A highly modified Heljan HO kit.

Below: Overview of the loco service area (east side of the layout) with Sandi looking on.





Bill Winans Photo



Above: Updated shot of Coal Creek, with things a bit more cleaned up than when you were there. The little engine house is another of Gerry Evans' structures. Beyond it, but hidden from view, is a hand car shed he also built. Other than the main lines, much of the layout's track design was dictated by the buildings Gerry Evans gave Bill to use, and Bill is very grateful to him for all of them.

Left: Engine house with several narrow gauge locos.



Bill Winans Photo

Bill sent us a new view of the overview of Coal Creek that is also a bit more current than when we were there in December.



K-28 2-8-2 in Coal Creek. The water tank was built by Gerry Evans, but modified by Bill.



Catapano Coal Tipple (under construction) in Coal Creek.

Bill Winans Photo



Coal Creek freight shed for public use, also built by Gerry Evans. Bill cut it down a bit to be more in tune with the height of narrow gauge cars.



Above: An overview of the north side of the layout shot from the east.

Below: Overview of the south side of the layout with Coal Creek on the upper level.



Next page top: An overview of the east side of the layout showing all the engine facilities and the industrial area in the background.

Next page bottom: An overview of the north side of the layout shot from the west. In the foreground is the narrow gauge mainline subgrade.



Please Note: These two images were taken with a panorama setting with distorts some straight lines.

Bill's trackwork is impeccable.





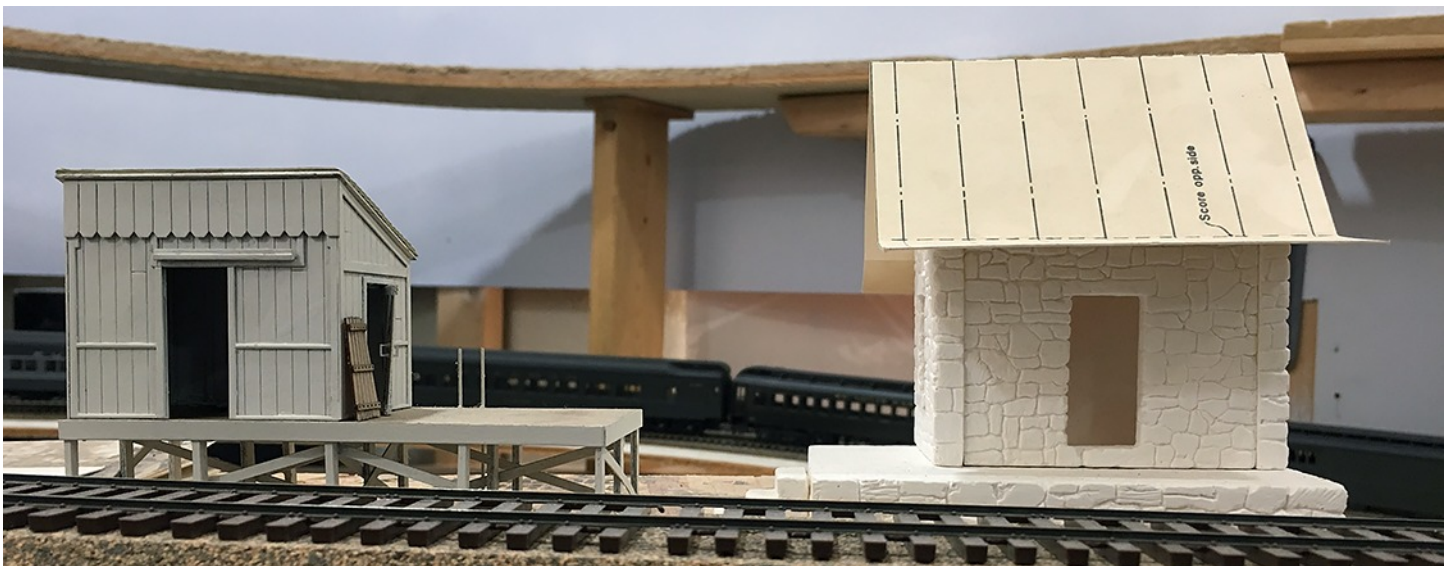


*Above: The SWM 4-4-2 with the tender from the Omnicon Erie 4-6-0 and the RRM 4-8-2;
both pulling the overnight mail.*

Below: Oriental Limited SW-1 with NWSL Stanton power trucks.



Madison Park siding with stone station and freight shed built by Gerry Evans.





Overview of the NE corner of the layout.



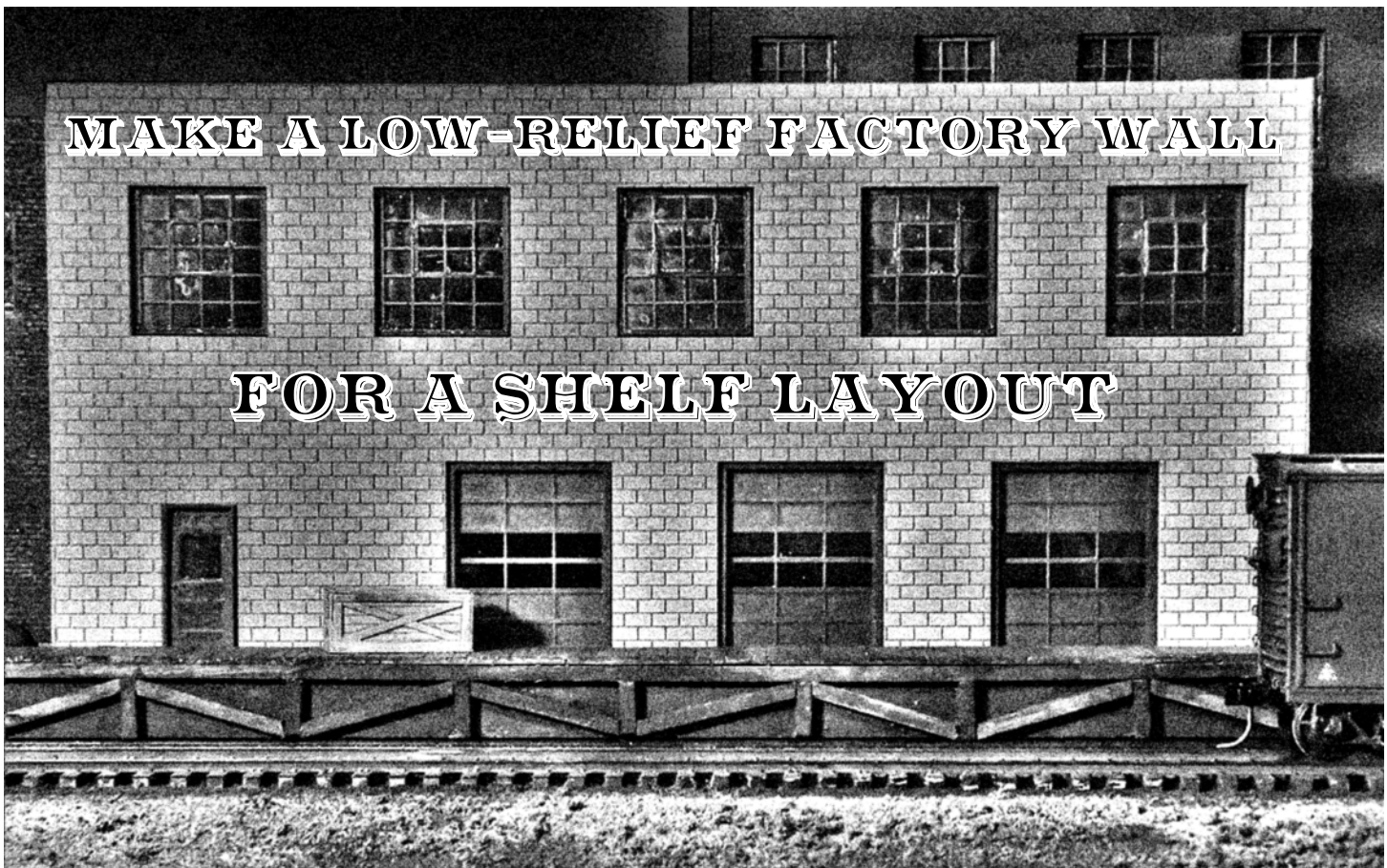
I know a few O Scale people as well who still use these.



Above: Bank of Caboose ground throws remotely located from their turnouts.

Below: Our very gracious hosts, Sandi and Bill.





By Mark Charles

Introduction

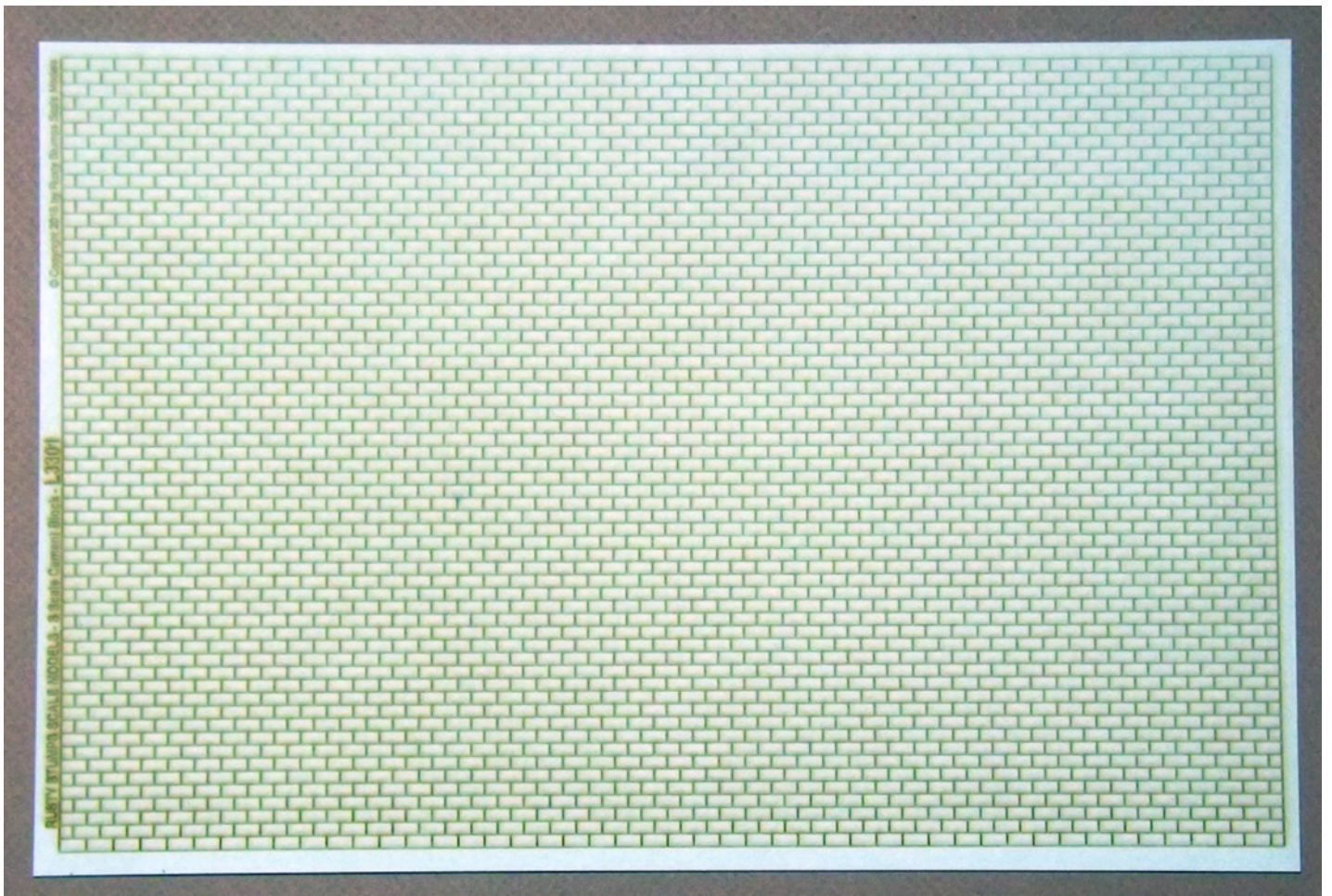
Shelf layouts are gaining popularity. After all, real railroads are very linear. Building a layout on a relatively narrow shelf allows the layout to coexist with other uses of the room (for example, as an office or guest bedroom). The concept supports progressive construction, where one section is completed before starting the next. Best of all, shelf layouts can be readily moved if the owner must relocate due to employment or retirement.

One challenge of a shelf layout is that often the tracks are relatively close to the backdrop. To scenic such areas, it can be useful to build partial structures, with a long wall facing the tracks, but very short side walls. When outfitted with doors and freight platforms, these structures can represent industries that ship or receive by rail, thus adding operational interest while using relatively little space.

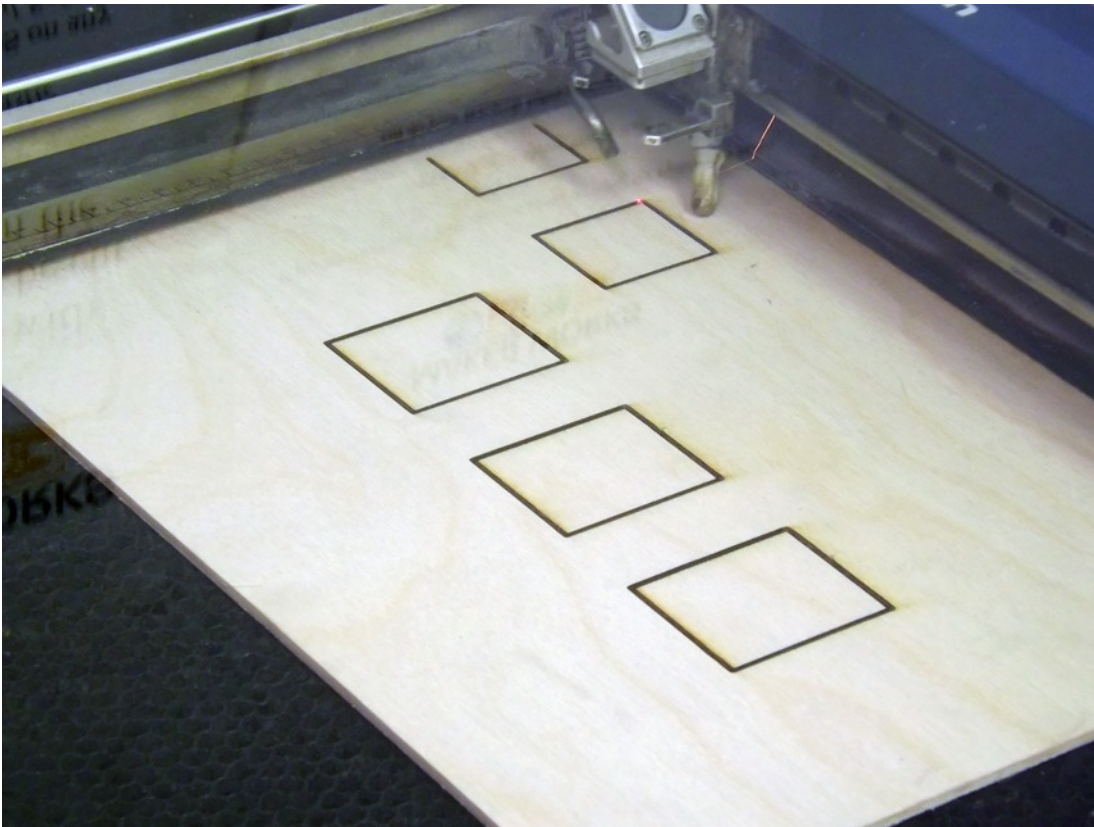
This article presents a technique for scratch-building such structures. It describes alternative methods and materials that may be helpful to anyone building a shelf railroad.

Procedure

The first step is to find a drawing or photo of the track-facing side of a structure. For this example, I'll use a two-story structure of cement block construction with metal-framed windows. Such structures were commonly built in the 1900's for factories, grocery warehouses and similar purposes. A side elevation was drawn using a free computer program called Inkscape. (As noted in a previous article, the Model Builder program from Evan Designs is also an excellent tool for making mock-ups and revising them before committing more expensive materials.) The drawing can be [downloaded here](#).



Rusty Stumps L3301 S scale Cement Block Wall Sheet

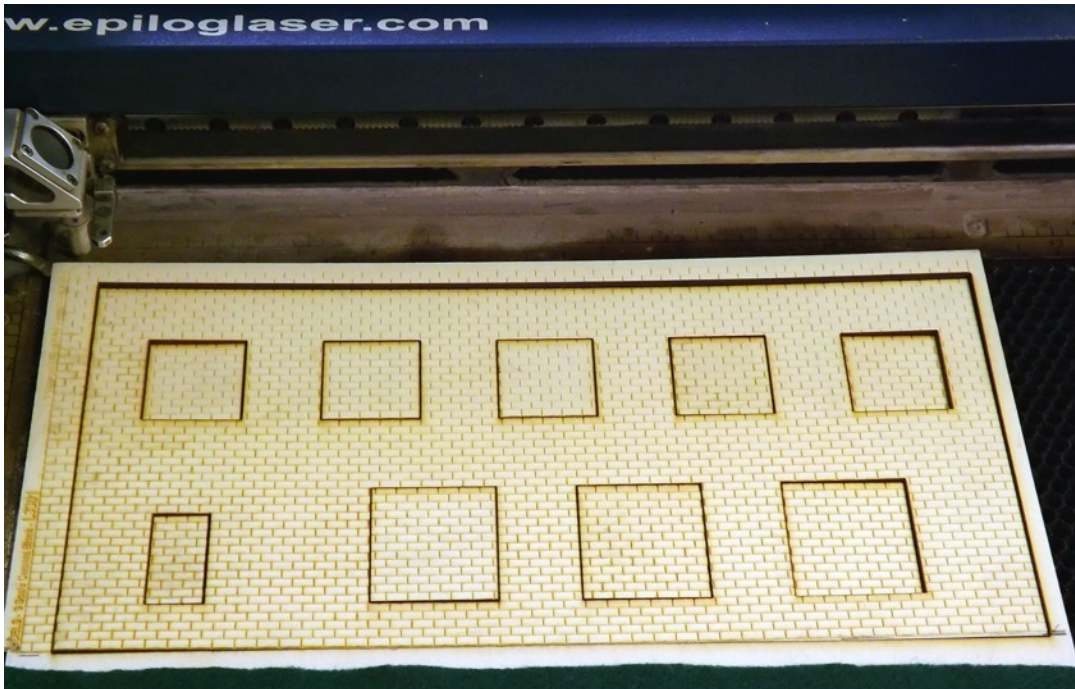


Laser cutting door and window openings. This can also be done by hand.

The second step is to assemble the materials. This example uses plastic windows and doors from Tichy Train Group. The cement block siding material is from Rusty Stumps Scale Models; the 0.016 RC-board is recommended. The subwalls are thin plywood, but styrene can be substituted. (See bill of materials).

Construction begins by cutting the walls and subwalls to size, and by cutting openings for windows and doors. If a laser cutter is available, this can be done directly from a scale drawing. Check with local com-

munity colleges or Makerspaces (see the end of this article for information) to see if they have a laser cutter available. Otherwise, use a nibbling cutter (Micro-Mark 81477) to cut the openings. Test fit windows and doors, and file or shim openings as needed.



Above: The laser cuts very precisely, so you can cut walls and subwalls separately and they will align perfectly.

Below: Test fit windows and doors. If you cut walls by hand, it may be necessary to file or shim the openings.

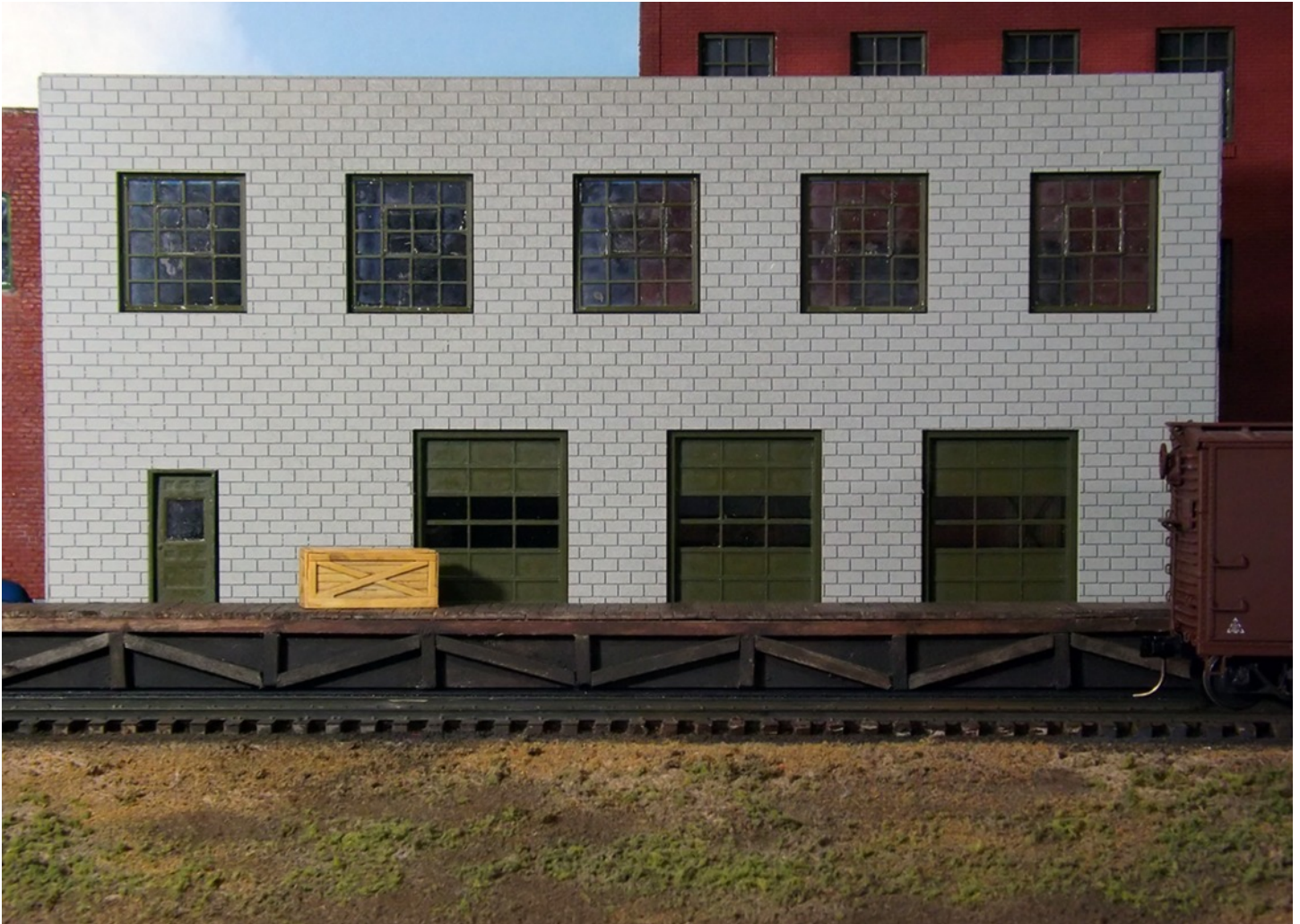


Assemble the cement block material to the long (track-side) subwall. Add the short (side) subwalls, butting the edge of the sidewall to the back of the long wall. Clamp and allow the adhesive to cure. When gluing siding to the side walls, align the corners so each half block adjoins a full one. This simulates the rectangular blocks making up the corner. Brace the long wall horizontally, providing for interior floors if you wish. Corner braces are helpful. Install cap tiles (coping) and a narrow flat roof to complete the basic structure.

Painting and Detailing

From the 1960's until today, real-world commercial buildings are usually spray painted, so blocks and mortar are the same color. Metal doors and window frames are painted separately in a contrasting color. Use your favorite techniques to paint your model. The cement block material accepts water-based or solvent-based paint. It's usually best to paint doors and window frames before assembly.

Detailing will make your structure more realistic. Add a freight platform so railcars can be loaded or un-



Painted and detailed, ready for the layout.

loaded. Consider roof details, rain spouts, ramps, awnings and steps. Mount the structure at trackside. Use an NASG gauge to be sure that there's clearance for rolling stock. Now you have a new destination when switching.

Bill of Materials

- 2 pcs. Rusty Stumps L3301 S scale Cement Block Wall Sheet (0.016 RC board is recommended)
- 5 pcs. Tichy Train Group 3519 S scale 20 Pane industrial Windows
- 1 pc. Tichy Train Group 3514 S scale 1 Lite Door & Frame
- 3 pcs. RIX Products Smalltown USA 699-0004 Overhead Freight doors
- 4 pcs. Pikestuff 1008 Cap Tiles
- Plywood (1/8 inch) Micro Mark 50197 or styrene (0.060) Micro Mark 80903: 1 pc. 12" x 6", 2 pcs. 2" x 6" 1 Pc. 2" x 12"
- 1/2 inch square basswood or styrene for bracing

About Makerspaces

Communal workshops where hobbyists can share ideas and tools are popping up across North America, in schools, libraries, and community centers. Called makerspaces, hackerspaces or innovation labs, they offer training and access to computerized tools that would be difficult for an individual to purchase and maintain. According to *Popular Science* magazine, February 2016, there are more than 480 in North America.



Possible Makerspace layout.

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WHAT'S ON YOUR WORKBENCH TODAY?

This series shows our readers what other modelers are working on, and we need your help to make it successful. All that's needed is a simple snapshot of what your workbench looks like and the project on it. Send us a picture or two along with a short description of what you are working on so we can share it here. If it's a project under construction, send it in. Repair job, send it in. Completed project, send it in. Send your pictures and descriptions to daniel@modelrailroadresource.com

By Karl Johnson

Here are two models on the “workbench”, or in my case, the “desk top”.

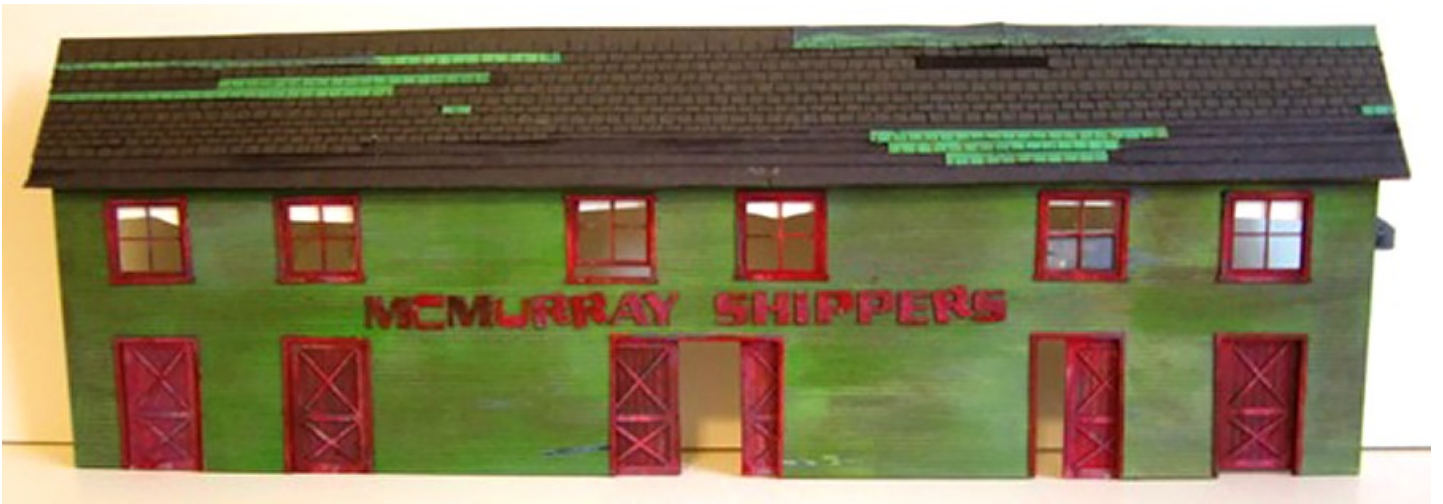
One is a Back Alley structure from Pine Canyon, the other is McMurray Shippers from Model Tech Studios. Both are designed to be placed up against a wall.





For the Pine Canyon model, I first painted the structure grey, and then with a paper towel added the red to the bricks, paying attention the degree of red so there would be a lot of unevenness on the brick work. I added a canopy over one doorway, mainly to keep things from dropping from the upper floor window directly onto the help on the dock. A side vent was added (Scenery Unlimited detail as I recall).





The McMurray building came with a paper roof which had to be cut out, but I had left over materials from other projects, and although I wish I had found all the extra materials first, there is a variety of roof materials patched into this roof.

WHAT'S ON YOUR WORKBENCH

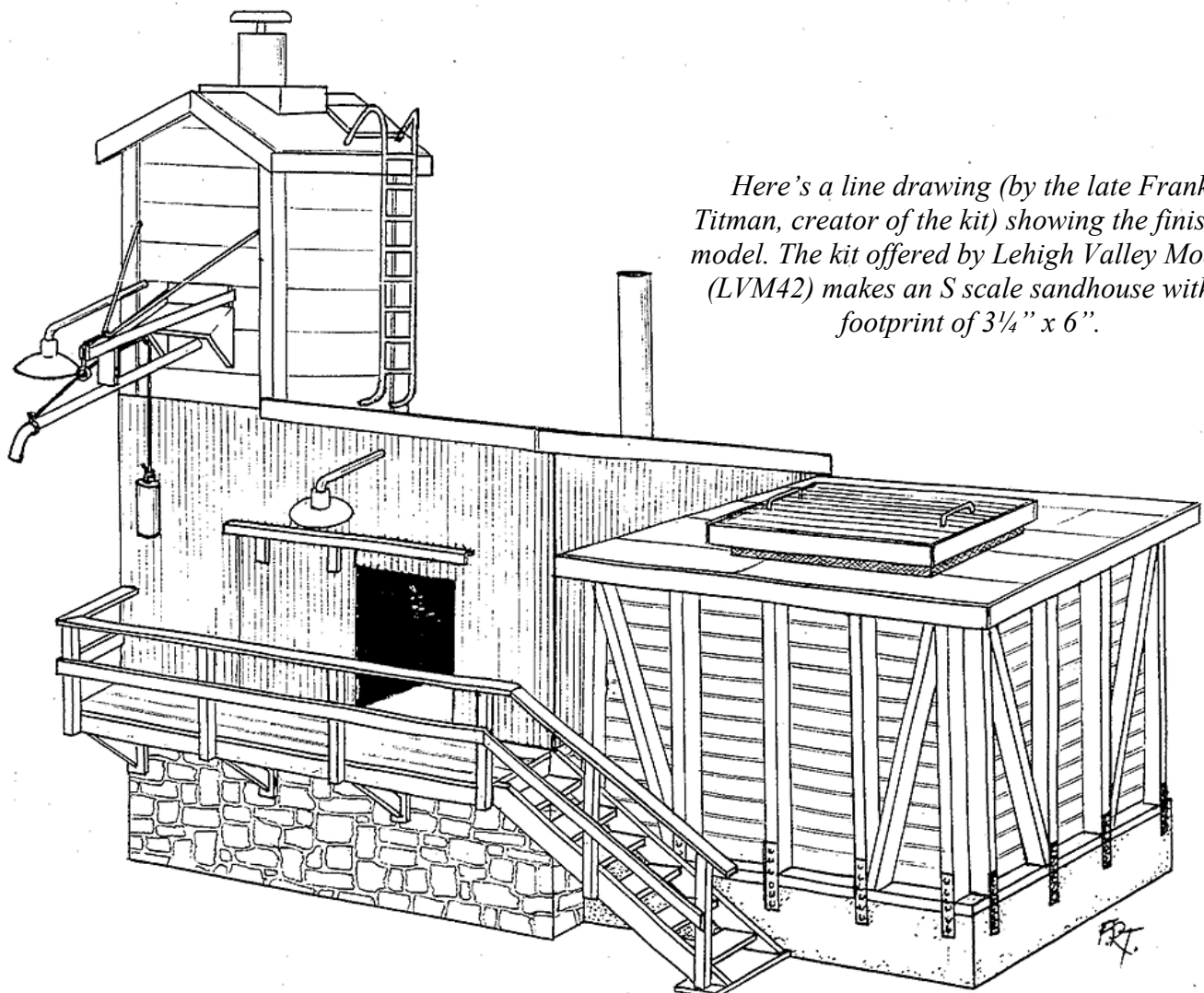
EXTRA

LEHIGH VALLEY MODELS SANDHOUSE BY GAYLORD GILL

The project on my workbench is the sandhouse offered by Lehigh Valley Models (kit #LVM42). Most everyone is familiar with the use of sand as an aid to good traction under a locomotive's driving wheels. Each locomotive terminal typically had a sandhouse, within which sand could be stored, heated to remove moisture and delivered to the loco. The instructions indicate this is a B&O prototype, but I think the model could easily be incorporated into the servicing facilities of other railroads.

The Sandhouse Kit

As with most of the LVM line, this kit would be considered old-school design – materials consist of cardstock, basswood, paper, a few metal castings and miscellaneous accessories. Some of the included accessories are pretty elemental, such as a thumb tack to be used as a vent cover and a wooden dowel to represent a sheet-metal stovepipe. Some of the cardstock and basswood pieces need to be cut out by the modeler. Still, with a little patience and care, this type of kit can be assembled into a really nice model.



Here's a line drawing (by the late Frank Titman, creator of the kit) showing the finished model. The kit offered by Lehigh Valley Models (LVM42) makes an S scale sandhouse with a footprint of 3¼" x 6".

This article won't be a step-by-step construction piece, especially since I'm only partway done, but I will mention a few modifications I'm making to the sandhouse kit, as well as some construction techniques I recommend.

All-Square

For me, one thing that really detracts from a good model structure is any angle that is supposed to be 90 degrees, but isn't. Unless you're constructing some special type of structure, walls should be plumb, and adjacent walls should meet at a right angle.

One of the first steps in assembling this kit is to cut out a large sheet of .050" cardstock into the pattern shown in Photo 2. This single sheet forms the four walls of what's called the tower building, although the cardstock is not the finished exterior – various veneers are later added to represent stone, corrugated metal and tarpaper. The instructions tell how to score the corner seams with a blade so the walls will fold easily. Each of the corner seams, as well as the openings to be cut for the doors and windows, are already drawn on the cardstock by the manufacturer.

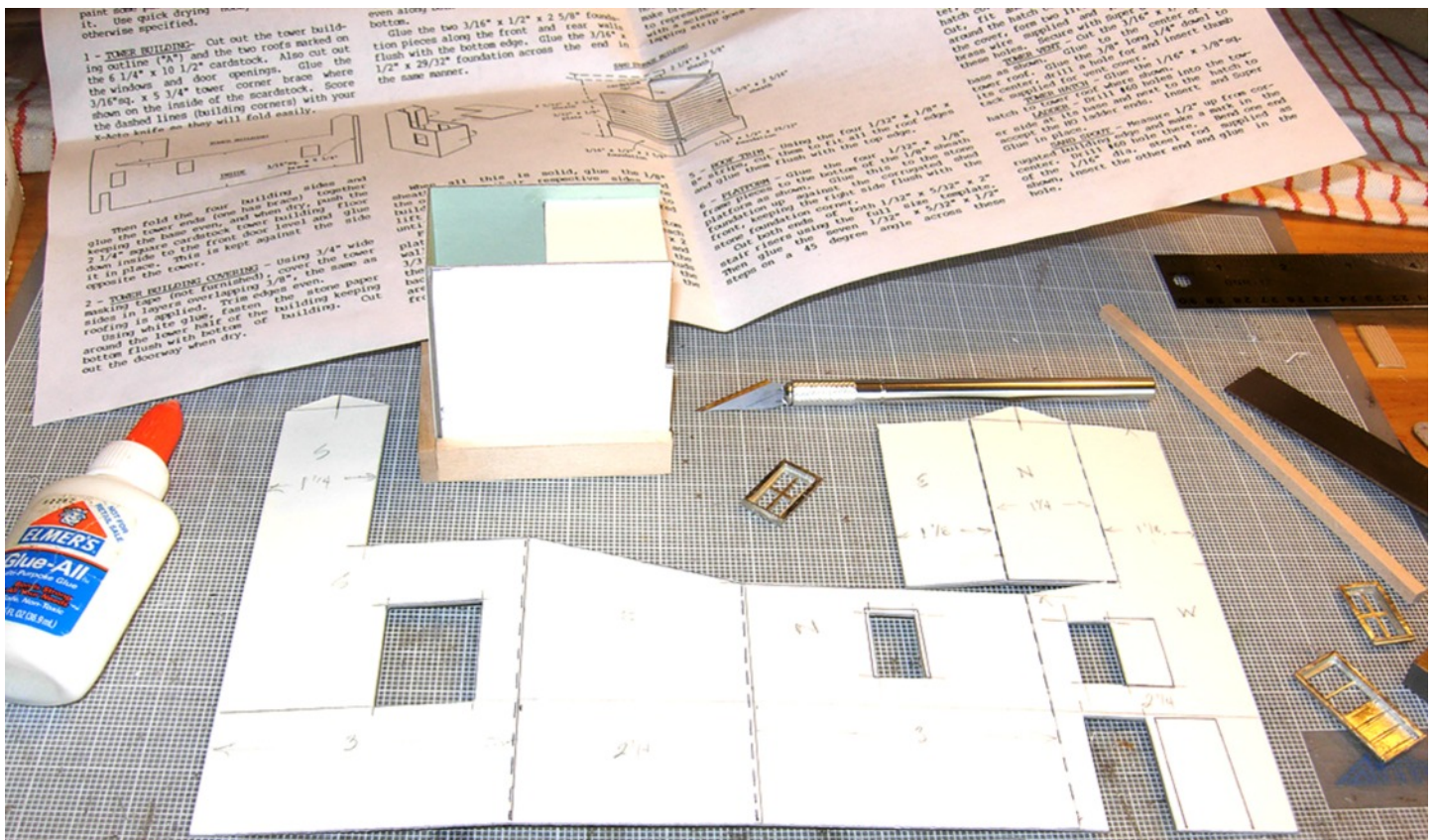
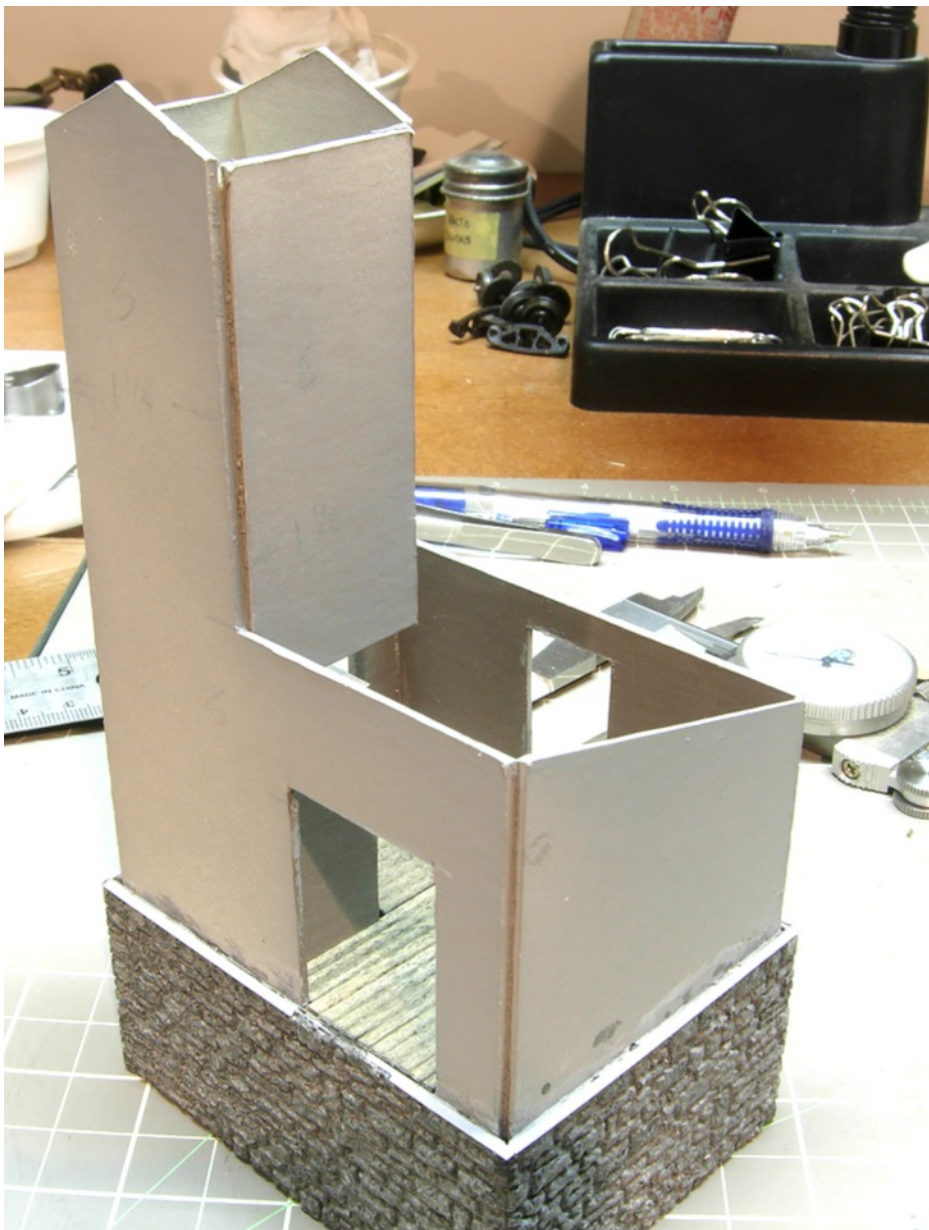


Photo 2: The core for the tower building is a single sheet of .050" cardstock. The modeler cuts out the outline, then scores along the corner seams so the structure can be folded into its four walls.

In looking at the vertical guide-lines, I felt at least one of them wasn't accurately drawn at right angles to the building base. A machinist's square confirmed this, so I realized I would have to redraw one or more lines. I then went a step further and considered whether the vertical lines would generate a structure which is a true rectangle – that is, were the north and south walls equal to each other in width, and similarly for the east and west walls. I again found some discrepancies, and I wound up redrawing 4-5 of the lines. Once I felt confident the template had accurate guide-lines, I made my cuts in the cardstock. I scored the corner seams pretty deeply, but left enough material that the walls didn't separate from each other. I folded the four main walls, and to help ensure the corners were square, I stood the structure on a cutting board with a grid marked on its surface. I then



glued the two free ends together and clamped everything until the glue dried. In a separate step I folded and glued the four walls which form the tower itself (Photo 3).

Photo 3 : Some careful checking of dimensions helped Gaylord form the tower building into a structure that is “all-square” – corner junctures are right angles and the walls are plumb.

Adding a Foundation

There are two components to this model: the tower building (to the left in Photo 1) and the sand storage building (with all the outside bracing). While the storage building includes a foundation, the tower component does not. I decided it would be worthwhile creating the second foundation, both for looks and also to help keep the tower building in its rectangular shape. From my home supply, I cut a piece of 3/16” basswood to the exterior dimensions of the tower building. I then glued onto it four alignment guides to create a nice fit against the inside walls of the structure. This is my preferred technique for structures – it allows the foundation to be anchored to the layout while the structure can be lifted off any time there is a need for maintenance.

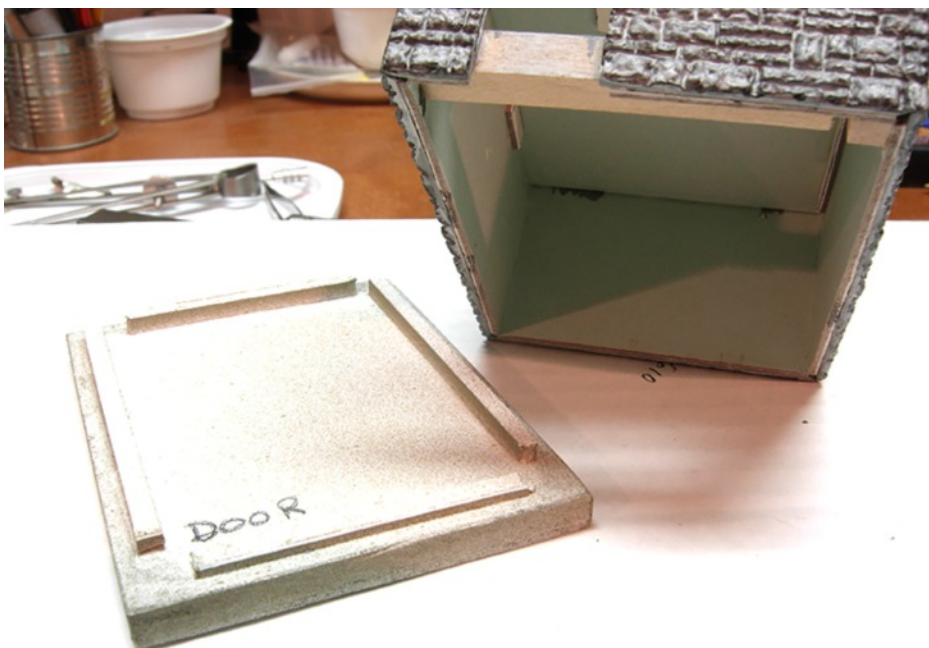


Photo 4: The sandhouse kit did not include a foundation for the tower building, so Gaylord created one out of basswood. The alignment keys allow later removal of the structure for maintenance.

I made a change of materials regarding the lower walls of the tower building. Included in the kit is a sheet of paper printed with a pattern representing stones and mortar-lines. This was intended to be glued around the bottom of the walls, on the outside of the cardstock. I preferred a material with the stones showing in relief, so I again went into my home supply and found a piece of molded plastic sheet by Vollmer, (Part no. 6031) (Photo 5).

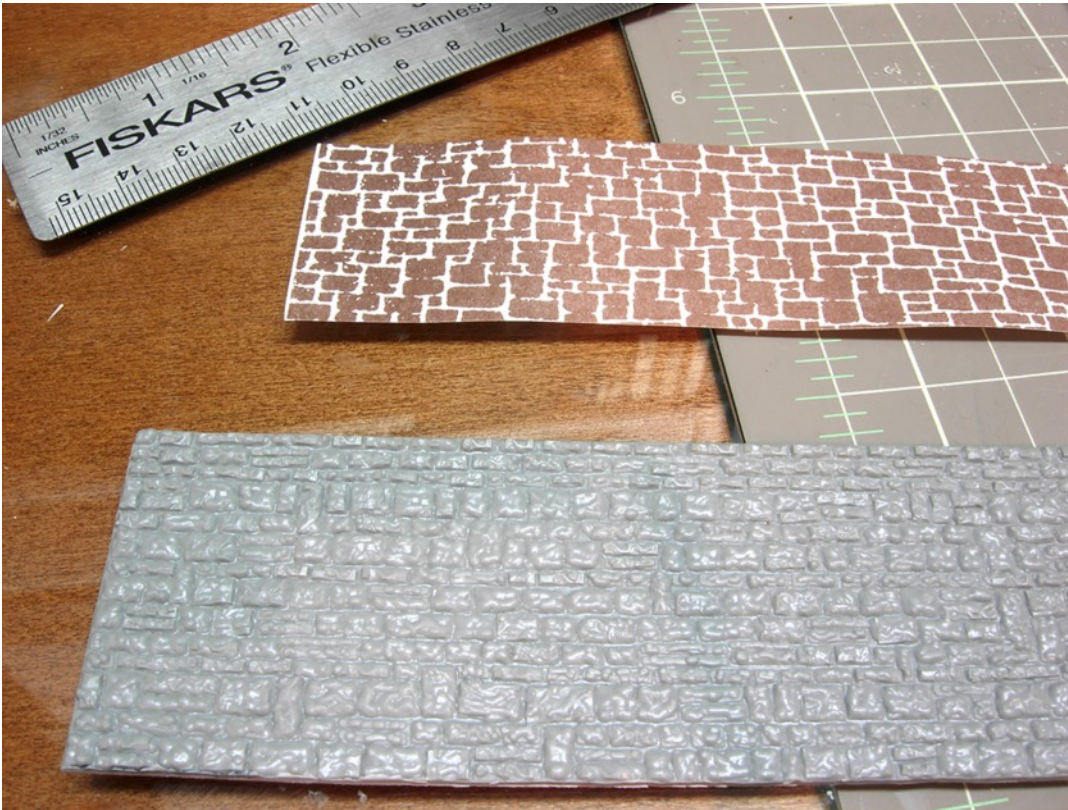


Photo 5: The kit includes a printed paper sheet to represent stonework at the lower part of the tower building. Gaylord replaced the paper with a plastic sheet by Vollmer, which shows the stones in relief.

One trick in using a 3-D material such as this: if you cut the wall pieces in succession, (say, starting at the left edge of the stone sheet and working in a counter-clockwise direction around the structure), you can get each of the adjacent walls to mate at the corners with the same mortar-lines. It does require that you cut away the insides of the vertical cuts in 45-degree bevels. You'll also need to do a little notching into the mortar line right at the seam, and maybe a bit of gap-filling with some putty. When done right, the stones at the edges wrap naturally around the corners (Photo 6).



Photo 6: The plastic stonework sheet, already painted and weathered in this view, effectively conveys the 3-D effect Gaylord was looking for. With additional work at the edges, the stones appear to wrap around the corners.

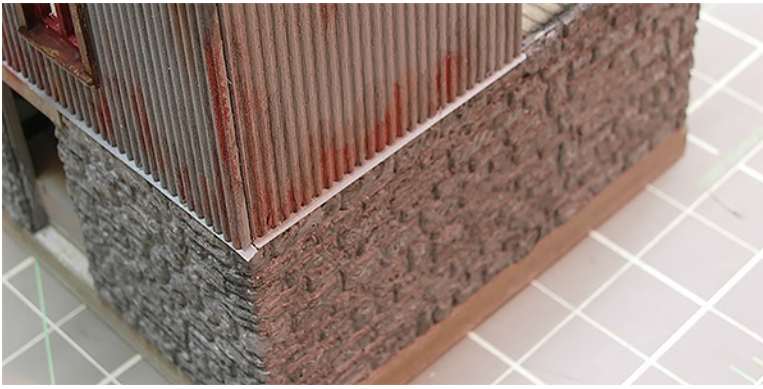


Photo 7: This close-up shows the paper strips Gaylord added between the corrugated metal walls and the lower stonework, representing flashing. The plank floor is another added detail.

The change in materials for the stonework caused one other small modification. Since the walls above the stonework will be corrugated metal, and the thickness of my 3-D stone sheet (.080") was much greater than the supplied paper sheet, the juncture between the two materials didn't look right. I figured what was needed was some flashing above the stonework to direct rain water away from the juncture. I accomplished this by spray-painting a sheet of paper on both sides with gray primer, cutting a few thin strips of it (about 3/32" wide) and then gluing the strips along the top of the stonework. (Photo 7)

Painting and Weathering as You Go

As with many structure assemblies, the instructions for the sandhouse kit suggest to paint some parts before gluing them in place. In this kit, that especially applies to the basswood (both strips and scribed sheets) that form the outer walls of the sand storage building. I wanted to stain these with an oil-based stain to represent wood which had been treated with creosote. Gluing those pieces together first would have created the possibility of getting glue on a visible part of the basswood, preventing the stain from being absorbed there. To create additional texture, before the staining step I used a razor saw to scribe grain into the basswood pieces (Photo 8).



I often think in terms of sub-assemblies where the entire sub-assembly can be painted at once. For example, I found it convenient to first build the wall assembly for the tower building, and to then add the stonework, before spray-painting that component inside and out with gray primer. Common hardware store spray cans are fine for this. Even though my Vollmer stone sheet material was already a gray color, the flat primer removed any of the shininess inherent in the plastic. I also used the same paint to cover both sides of the corrugated metal walls, which were made from milled basswood sheets.

Photo 8: The sand storage building is constructed of basswood sheets and stripwood. Gaylord scribed in some grain with a razor saw, then stained the pieces individually to simulate creosoted wood.

Sometimes, I also find it convenient to do some weathering at this stage. For the tower building, I wanted to contrast the stonework with the metal siding that would be just above it. I prepared a lighter-gray wash with acrylic paints and brushed it on each of the stone walls. With field-stone walls, I often paint individual rocks a variety of subtly different colors; however, my stone sheet seemed to represent quarried stones which would be more uniform in color. Finally, I brushed the walls fairly heavily with an alcohol/India ink mix.

I also wanted to weather the corrugated metal walls before installing them. I used Doc O'Brien's Weathering Powders to create a few rusty areas, employing the technique of a small paint brush dampened with alcohol and then dipped in the powder. Later, I again used the India ink mixture on these walls, but I did very light passes on select areas, almost a dry-brush approach. I stumbled on one technique here – the darker weathering at the top of the walls was the result of pressing the brush a little harder right at the top edge while holding the wall vertical, and the ink trails kind of tapered off in a fashion that seems realistic to me (Photo 9).

Still lots to do, but I wanted to share my progress so far.

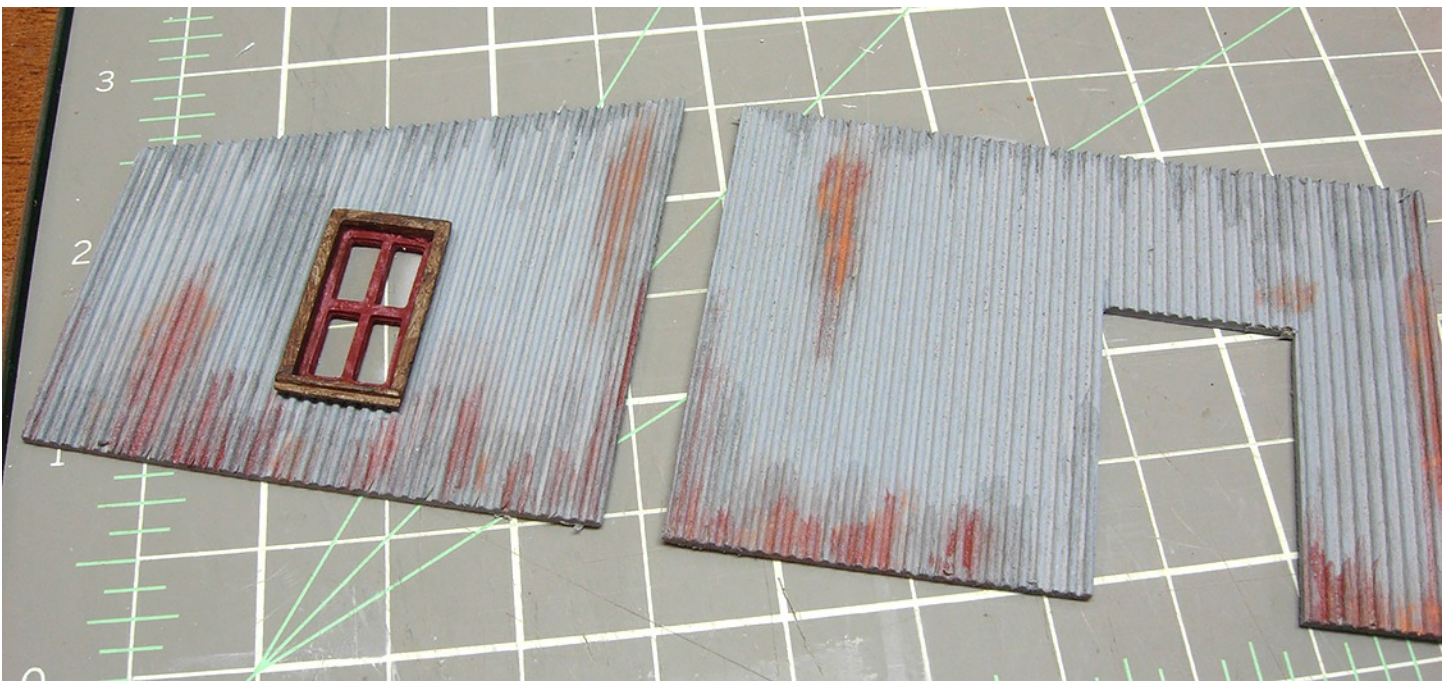


Photo 9: The upper walls are corrugated metal, represented by milled basswood. Gaylord spray-painted the wall pieces with flat gray primer, then weathered them with powders and washes.

S SCALE SHOWS & MEETS

The S Scale Resource Magazine will now be providing a free listing of upcoming events. This small, text only listing will include the Event, Date, Location, Type of Event, and Contact Information. [Click here](#) to go to the sign up form. This form will take your information, and we will publish it in our next issue. If it is an annual event, you will need to submit your information every year.

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(Be sure to use the "NASG17" code to get a reduced room rate).

[Click here](#) to visit the website, which includes the registration and car-order forms, and the tours.



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For the past 15 years, it has been chaired by James Canter, and he has decided it is time to "pass the torch" We, at The Model Railroad Resource LLC, publishers of The O Scale Resource and The S Scale Resource, are proud to have been selected to carry on the tradition for the 49th year, and include S Scale.

Website: indyoscaleshows.com

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